



## SHEFFIELD CITY COUNCIL Cabinet Highways Committee Report

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**Report of:** Executive Director, Place

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**Date:** 12 JULY 2012

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**Subject:** ECCLESALL ROAD SMART ROUTE – OUTCOME OF TRAFFIC  
REGULATION ORDER CONSULTATION PROCESS

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**Author of Report:** David Whitley

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**Summary:** The report sets out the response to advertised amendments to loading and waiting restrictions on Ecclesall Road, Ecclesall Road South, Moore Street (Charter Row side) and associated side roads.

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### Reasons for Recommendations

The Council has carried out extensive survey work and a comprehensive consultation exercise on Ecclesall Road. Based on the feedback, requests and information received, it is recommended to continue to progress with implementing the Ecclesall Road Smart Route, with implementing additional loading and waiting restrictions in three locations along the corridor being the next phase of the wider implementation plan.

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### Background Papers:

**Category of Report:** OPEN

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## Statutory and Council Policy Checklist

<b>Financial Implications</b>
NO Cleared by: Matthew Bullock
<b>Legal Implications</b>
NO Cleared by: Julian Ward
<b>Equality of Opportunity Implications</b>
None outstanding Cleared by: Ian Oldershaw
<b>Tackling Health Inequalities Implications</b>
NO
<b>Human rights Implications</b>
NO
<b>Environmental and Sustainability implications</b>
NO
<b>Economic impact</b>
NO
<b>Community safety implications</b>
NO
<b>Human resources implications</b>
NO
<b>Property implications</b>
NO
<b>Area(s) affected</b>
Central, Nether Edge, Ecclesall
<b>Relevant Cabinet Portfolio Leader</b>
Councillor Leigh Bramall
<b>Relevant Scrutiny Committee if decision called in</b>
Economics, Environment and Well-being
<b>Is the item a matter which is reserved for approval by the City Council?</b>
YES
<b>Press release</b>
NO

REPORT OF THE DIRECTOR OF  
DEVELOPMENT SERVICES

REPORT TO CABINET HIGHWAYS COMMITTEE

12 JULY 2012

ECCLESALL ROAD SMART ROUTE – OUTCOME OF TRAFFIC  
REGULATION ORDER CONSULTATION PROCESS

1.0 SUMMARY

1.1 The report sets out the response to advertised amendments to loading and waiting restrictions on Ecclesall Road, Ecclesall Road South, Moore Street (Charter Row side) and associated side roads.

1.2 The proposed changes were advertised using the feedback from two rounds of local consultation. This has helped develop an outline 'package' of interventions at fourteen locations along the corridor, the detail of which was reported to Cabinet Highways Committee (CHC) in December 2011.

1.3 The report seeks authority to implement the changes to loading and waiting restrictions on Ecclesall Road, Ecclesall Road South and associated side roads as shown in Appendix A. These changes would enable three of the fourteen interventions to progress.

1.4 The Ecclesall Road Smart Route is a jointly funded project between the City Council and South Yorkshire Passenger Transport Executive (SYPTX).

2.0 WHAT DOES THIS MEAN FOR THE PEOPLE OF SHEFFIELD

2.1 Previous consultation with residents, businesses and users of the corridor has taken place to develop proposals designed to improve bus and car journey times on the Ecclesall Road corridor. The next stage of the project is to start implementing changes associated with the first two phases of consultation and re-design some elements of the scheme to enable the public to respond to revised plans of the proposed interventions. The planned changes should make it easier for most users to travel along the corridor.

3.0 OUTCOME AND SUSTAINABILITY

3.1 The response to the consultation contributes to the 'working better together' value of the Council Plan "Standing up for Sheffield", with proposals that respond to customer comments about existing travel conditions along Ecclesall Road. The overall project contributes to the "sustainable and safe transport" objective with proposals to improve access to the public transport network, public transport journey time reliability and alternatives to the private car for some local journeys in Sheffield.

3.2 A key outcome of the report will be approve the implementation of loading and waiting restrictions on Ecclesall Road, Ecclesall Road South and associated side roads.

#### 4.0 REPORT

##### **Background**

4.1 The second Local Transport Plan (LTP) identified Ecclesall Road as a 'congestion target route' aimed at reducing personal journey times and providing better public transport. The main locations of journey delay were at Hunters Bar and Moore Street roundabouts, but local consultation helped develop an outline 'package' of interventions at fourteen locations along the corridor. More detail about the fourteen intervention areas are included in Appendix B.

##### **Traffic Regulation Order (TRO) Consultation**

4.2 The TRO containing the amendments were advertised from 16<sup>th</sup> December 2011 to 13<sup>th</sup> January 2012. Around 120 letters were distributed to properties most closely affected to highlight the proposed changes to the TRO. In addition, an email was sent to around 450 people who had expressed an interest in being kept informed about changes on Ecclesall Road. Finally, street notices were used to make people aware that plans were available in Howden House and on the internet.

4.3 A total of 27 responses were received during the TRO consultation period. There were:

- 13 comments supporting the changes
- 2 comments supporting the changes (with conditions)
- 4 comments objecting to the changes and
- 8 more general comments: with two asking for further alterations to the proposed restrictions.

4.4 This report forms the basis of the response to the comments made during consultation.

##### **Consultation analysis**

4.5 Traffic Regulation Orders were advertised in five different locations. **Location One: Ecclesall Road (outbound) between Marmion Road and Carrington Road.** Details of the planned restriction are shown in plan TM-BN721-07-TRO in Appendix A. The total length of double yellows proposed would remove around 10 parking spaces, which are currently available for use during the interpeak hours of 0930-1600 (Monday to Friday) and all day Saturday and Sunday.

4.6 There were six responses relating to this area, three supporting, two comments (accepting the rationale of the scheme, but requesting that

existing restrictions on the opposite side of Ecclesall Road be reduced) with one objection based on the reduction in parking opportunities in the area.

- 4.7 In previous consultation, nine respondents suggested parking restrictions at this section of Ecclesall Road as double yellow lines would provide a similar benefit to the originally proposed widening of the road at a much lower cost.
- 4.8 The suggestion made by two traders that the bus stop clearway on the inbound side of Ecclesall Road at Carrington Road be reduced is achievable. This could create up to three additional parking spaces in the area without the need to re-advertise a further Traffic Regulation Order. However, it would require moving a bus stop and shelter. This would involve additional local consultation and cost around £4500 to put in a raised kerb and tactile paving in the new location.
- 4.9 Although just a sample, weekday parking surveys showed that a maximum of 21 vehicles (50% of on spaces on both sides of the road) were parked on the section of Ecclesall Road between Marmion Road and Carrington Road. Although the number of spaces in this section would be reduced by around 10, there would still be around 32 spaces available on both sides of the road with 8 being on the outbound side.
- 4.10 Demand for the available parking spaces is higher on Saturdays - with occupancy being up to 75% on the outbound side of Ecclesall Road. However, the demand would appear to come primarily from short stay visitors rather than local residents as many residents choose to park behind their properties, on Marmion Road. In summary, there should still be reasonable spare parking spaces on either side of the road on Ecclesall Road.
- 4.11 It is recommended to progress with the implementation of the double yellow lines on the outbound side of Ecclesall Road between Marmion Road and Carrington Road and work with SYPTE to consult on creating extra parking spaces by moving the bus shelter and reducing the length of the bus stop clearway at the inbound Carrington Road stop.
- 4.12 **Location Two: Amendments to parking spaces on Ecclesall Road outbound (opposite Greystones Road) to maintain a through lane for traffic passing vehicles waiting to turn right into Greystones Road.** Details of the planned restriction are shown in plan TM-BN726-P2-TRO in Appendix A. The total length of double yellows would remove five spaces, which are currently available during the interpeak hours of 0930-1600 (Monday to Friday) and all day Saturday and Sunday.
- 4.13 There were three responses relating to this area, one objecting to the changes and two more general comments. The objection was based on the reduction in parking and loading and unloading opportunities in the area.
- 4.14 In previous consultation, thirty six respondents welcomed the proposals including the double yellow lines in the area. Although comments about the proposals were generally favourable (particularly about the

introduction of double yellow lines to reduce the effect that waiting traffic has on through traffic in the area), there were concerns raised about the affect on trade of additional waiting restrictions in the area. As it was not proposed to introduce loading and unloading restrictions in this area, loading and unloading will still be allowed on the double yellow lines outside the shops.

- 4.15 Although just a sample, weekday parking surveys have shown that a maximum of six vehicles (30% of on spaces on both sides of the road) were parked on the section of Ecclesall Road between Greystones Road and Carrington Road. Although the number of spaces in this section would be reduced by around five, there would still be around 15 available.
- 4.16 Demand for the available parking spaces is higher on Saturdays - with occupancy being around 75% on the inbound side of Ecclesall Road, primarily short stay visitors and not residential. The outbound side still has plenty of spare parking spaces.
- 4.17 Despite their being available parking opportunities further down the hill on the outbound side of Ecclesall Road, the proposed length of the double yellow line was reviewed in the response to the objection. It may be possible to achieve the same benefits at Greystones Road with a slightly reduced length of double yellow line. This would retain up to two parking spaces in the area near Greystones Road.
- 4.18 It is therefore recommended to progress with the Traffic Regulation Order to implement the full extent of double yellow lines on the outbound side of Ecclesall Road between Carrington Road and Greystones Road, but not actually implement a 10m section to retain a small amount of parking near the shops. However, should monitoring show that the retained parking still causes delays for 'through' traffic trying to get past the right turning traffic into Greystones Road, double yellow lines would be extended in place of the retained parking area. If this suggested approach is taken, the double yellow lines could be extended without the need to advertise an additional Traffic Regulation Order, but it would provide an opportunity to see if a small amount of parking could be retained in the area.
- 4.19 Details of the restriction to be implemented initially are also shown in plan TM-BN726-P2-TRO in Appendix A.
- 4.20 **Location Three: Providing right turning lanes off Ecclesall Road South into Gisborne Road and Ringinglow Road - so these vehicles wait out of the way of through traffic.** Details of the planned restriction are shown in plan TM-BN721-02-TRO in Appendix A. The total length of double yellows would remove around twenty spaces (five on Gisborne Road), which are currently available during the interpeak hours of 0930-1600 (Monday to Friday) and all day Saturday and Sunday on Ecclesall Road South and at all times on Gisborne Road.

- 4.21 There were six responses relating to this area, two supporting the proposals, three more general comments and one objection. The general comments were based on concerns that the right turn out of Gisborne Road onto Ecclesall Road should be banned and double yellows at the end of Gisborne Road would simply move the parking issues further up Gisborne Road. In addition, a resident of Ecclesall Road South wanted to make it easier to reverse into their drive while an employee representing Henry Boot contacted us concerned about the ease of leaving Banner Cross Hall towards town.
- 4.22 In previous consultation, forty nine respondents welcomed the proposals including the double yellow lines in the area whilst twenty eight residents thought that the scheme that these lines were part of would create additional congestion.
- 4.23 It is not intended to ban the right turn from Gisborne Road into Ecclesall Road South although this could reduce queue length on Gisborne Road for traffic turning left onto Ecclesall Road South. It would have a negative impact in terms of restricting access for residents and divert additional traffic on Ecclesall Road South's already heavily trafficked other junctions, such as Ringinglow Road and Bents Road.
- 4.24 Cyclists raised concerns about encouraging more vehicles to use the nearside lane, making conflict with slower moving cyclists more likely. Although an opportunity would be taken when remarking the Ringinglow Road junction to increase the inside (uphill) lane slightly for cyclists, it is proposed to continue to progress investigations into both a quiet road parallel cycle route to Ecclesall Road/Ecclesall Road South and a shared use cycle route using the existing footway on Ecclesall Road South too. Although the uphill footway in this area is not heavily used by pedestrians, with widths being less than 2m in places, providing a shared use (walking and cycling) facility would require investment and further local consultation in the area. A 'Keep Clear' marking will now be included in the detailed design to improve the egress from Banner Cross Hall.
- 4.25 The improved right turn lane from Ecclesall Road South to Ringinglow Road that would be 'enabled' through the advertised TRO should provide clarity of lane usage and improving road safety through reducing 'weaving' between lanes.
- 4.26 It is therefore recommended to progress with the Traffic Regulation Order to implement the full extent of double yellow lines on the outbound side of Ecclesall Road South between Gisborne Road and Ringinglow Road.

- 4.27 **Location Four: Ecclesall Road (outbound) at the junction of Blenheim Mews.** The total length of new double yellows involves the removal of around four spaces, which are currently available all day every day.
- 4.28 There were eight responses relating to this area, seven supporting the proposals and one more general comment about wanting additional restrictions, which could not be delivered without a revised Traffic Regulation Order being advertised. As there were no objections to this part of the order, the order was approved using delegated authority and was 'sealed'. Works are due on site imminently.
- 4.29 **Location Five: To reduce queues for all vehicles approaching Moore Street roundabout, we propose to change lane markings on this approach and on the roundabout itself.** This would make it easier for all traffic to move into the correct lane and travel around the roundabout.
- 4.30 In previous consultation, 65 respondents provided feedback about the proposals for Moore Street Roundabout, with sixteen respondents welcoming the proposals, while another eleven made comments about the road markings in the area. One issue that was raised during the April 2011 wider scheme consultation was the need to plan for a two lane exit onto Moore Street/Charter Row – particularly in advance of the New Retail Quarter This would be achieved through removing a short section of build out, but would also require removal of an underused on street parking area for about 10 cars in front of the electricity substation. At the same time, removing the 'offside' section of build out will enable easier bus access to the bus lane on the approach to Fitzwilliam Gate.
- 4.31 There were no objections to the Traffic Regulation Order to remove the parking area, so following approvals obtained from CHC in December 2011, the order was 'sealed' and the scheme was implemented in March 2012.

#### **Relevant Implications**

- 4.32 Subject to members' approval, it is proposed to fund the anticipated £8,000 cost of implementing the signing and lining changes associated with this Traffic Regulation Order using the 2012/13 Local Transport Plan (LTP) allocation. Although the outline LTP allocations were approved at CHC on 26<sup>th</sup> April 2012, individual schemes within the programme still need Cabinet approval, which will be obtained through the CAF process. There are no legal implications associated with this report.
- 4.33 A full Equality Impact Assessment (EIA) has been undertaken for the Ecclesall Road Smart Route scheme and was reported to CHC on 8<sup>th</sup> December 2011. It concludes that the actions proposed are equality neutral in most cases although they may have some low level negative effects on certain groups (e.g. elderly, disabled). An action plan has



therefore been prepared to mitigate these impacts where possible and is set out in the full EIA document.

## 5.0 ALTERNATIVE OPTIONS CONSIDERED

5.1 Officers considered the degree of outline support for the proposals and the content of each individual comment received. Not implementing these proposed interventions is an option, but would be contrary to 'working better together' value of the Council Plan "Standing up for Sheffield"

5.2 Other options considered included widening Ecclesall Road (outbound) slightly between Rustlings Road and Greystones Road. The change in kerb location would involve significant costs associated with moving statutory undertakings plant. Implementing parking restrictions in this section rather than widening provides the same benefit for much less cost.

5.3 In terms of not carrying out the implementation of changes advertised in the Traffic Regulation Orders, doing nothing is an option, but would lead to a continuation of a less effective use of highway capacity along the corridor.

## 6.0 REASONS FOR RECOMMENDATIONS

6.1 The Council has carried out extensive survey work and a comprehensive consultation exercise on Ecclesall Road. Based on the feedback, requests and information received, it is recommended to continue to progress with implementing the Ecclesall Road Smart Route, with implementing additional loading and waiting restrictions in three locations along the corridor being the next phase of the wider implementation plan.

## 7.0 RECOMMENDATIONS

7.1 To overrule the objections and make the Traffic Regulation Order in accordance with the Road Traffic Regulation Act 1984.

7.2 To seek approval to make and implement the Traffic Regulation Orders as shown in plans TM-BN721-07-TRO, TM-BN726-P2-TRO and TM-BN721-02-TRO in Appendix A

Simon Green  
Executive Director of Place  
12 July 2012

**Appendix A: Ecclesall Road Smart Route - revised loading and waiting restrictions**

Available on separate plans.

## **Appendix B: A brief summary of the 14 interventions along Ecclesall Road.**

B1. Those interventions that are 'enabled' by approving the recommendations in this report (numbered 3, 7 and 8) are highlighted in italics.

- **Intervention 1:** Bents Green local shopping area. Highlighting that planned bus stop, crossings and parking management improvements would not be progressed.
- **Intervention 2:** At the junction with Knowle Lane, providing a new bus shelter closer to the road, allowing people to wait in more comfort and still see approaching buses.
- ***Intervention 3:*** *Providing right turning lanes off Ecclesall Road South into Ringinglow Road and Gisborne Road - so these vehicles wait out of the way of through traffic.*
- **Intervention 4:** Mini-roundabouts on Ecclesall Road South at Brincliffe Edge Road. Highlighting that such a facility would significantly affect traffic flow along the road and as such will not be progressed.
- **Intervention 5:** Moving the inbound bus stop at Glenalmond Road slightly down hill would allow traffic to flow more freely by reducing the frequency that a stopped bus blocks other traffic
- **Intervention 6:** Making the outbound bus lane on the approach to the traffic lights at Psalter Lane slightly shorter.
- ***Intervention 7:*** *Amendments to parking spaces on Ecclesall Road outbound (opposite Greystones Road) to maintain a through lane for traffic passing vehicles waiting to turn right into Greystones Road. Moving the inbound bus stop to the north side of Greystones Road to maintain a through lane for traffic passing vehicles waiting to turn right into the Co-op.*
- ***Intervention 8:*** *Highlighting that the costs associated with widening Ecclesall Road (outbound) slightly between Rustlings Road and Greystones Road would mean that it would not be progressed, but a suggestion to achieve the same benefits through new loading and waiting restrictions will.*
- **Intervention 9a:** Amending the approach to Hunters Bar by removing the inbound and outbound bus lanes between Hunter's Bar and Rustlings Road and improving the pedestrian crossings on the roundabout itself.
- **Intervention 9b:** Amending Hunters Bar roundabout to provide two 'proper' lanes on the roundabout and improved lane markings on the roundabout approaches.
- **Intervention 9c:** Amending Hunters Bar roundabout to improve pedestrian crossing facilities on all sides of the roundabout, widening the islands to make more space for pedestrians crossing and moving the crossings slightly so that traffic is less likely to queue back onto the roundabout.

- **Intervention 10:** Amendments to parking spaces on Ecclesall Road (from Summerfield Street to Hunters Bar) to ease right turning movements (particularly outbound) off Ecclesall Road and lengthening or moving a small number of bus stops.
- **Intervention 11:** A new pedestrian controlled crossing near the Nursery Tavern – implemented and funded as part of a Marks and Spencer’s retail development on the former Evans Halshaw site.
- **Intervention 12:** Lengthening the merge on Ecclesall Road (outbound) beyond Summerfield Street.
- **Intervention 13:** To reduce queues for all vehicles approaching Moore Street roundabout, we propose to change lane markings on this approach and on the roundabout itself
- **Intervention 14:** Marginal road widening and lane management improvements on Charter Row and Hanover Way approaches to Moore Street roundabout.

# APPENDIX A

## TM-BN721-02-TRO



New double yellow lines to ensure access and maintain visibility for motorists and pedestrians when entering/exiting the junction

N.B ALL EXISTING PARKING RESTRICTIONS ARE TO REMAIN AS EXISTING UNLESS SHOWN OTHERWISE

<b>KEY</b>
Proposed Double Yellow Lines (Prohibition of Waiting at Anytime with No Loading Mon - Fri, 7.30am-9.30am and 4.00pm-6.30pm)
Existing Double Yellow Lines (Prohibition of Waiting at Anytime with No Loading Mon - Fri, 7.30am-9.30am and 4.00pm-6.30pm)
Existing Single Yellow Line (Prohibition of Waiting and No Loading, Mon - Fri, 7.30am-9.30am and 4.00pm-6.30pm)
Existing Bus Stop Clearway

<b>DEVELOPMENT SERVICES</b> <b>TRANSPORT &amp; HIGHWAYS DIVISION</b> <b>TRAFFIC SECTION</b> 2-10 CARBROOK HALL ROAD SHEFFIELD S9 2DB Tel. (Reception) 0114-273-6320. Fax. 0114-273-6182 D Curtis B.A., Dip. T.P., M.R.T.P.L., F.I.H.T. Head of Development Services A Service Area of the Directorate of Development, Environment & Leisure Sheffield City Council	Client <b>SHEFFIELD CITY COUNCIL</b>	Drawing No. <b>TM-BN721-02-TRO</b>	SHEFFIELD City Council Development Environment & Leisure
	Scheme <b>Ecclesall Smart Route</b>	Scale <b>1:500</b>	Date Nov 2011
Drawing Title <b>Carterknowle Road to Ringinglow Road                  Traffic Regulation Order</b>	DRAWN ML Nov 11	CHECKED	DEVELOPMENT SERVICES

# APPENDIX A

## TM-BN721-07-TRO



Proposed double yellow lines as an alternative to previously proposed carriageway widening.

N.B. ALL EXISTING PARKING RESTRICTIONS ARE TO REMAIN AS EXISTING UNLESS SHOWN OTHERWISE

<b>KEY</b>
Proposed Double Yellow Lines (Prohibition of Waiting at Anytime with No Loading Mon - Fri, 7:30am-9:30am and 4:00pm-6:30pm)
Existing Double Yellow Lines (Prohibition of Waiting at Anytime with No Loading Mon - Fri, 7:30am-9:30am and 4:00pm-6:30pm)
Existing Single Yellow Line (Prohibition of Waiting and No Loading, Mon - Fri, 7:30am-9:30am and 4:00pm-6:30pm)

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 Development Services

A Service Area of Place  
 Sheffield City Council

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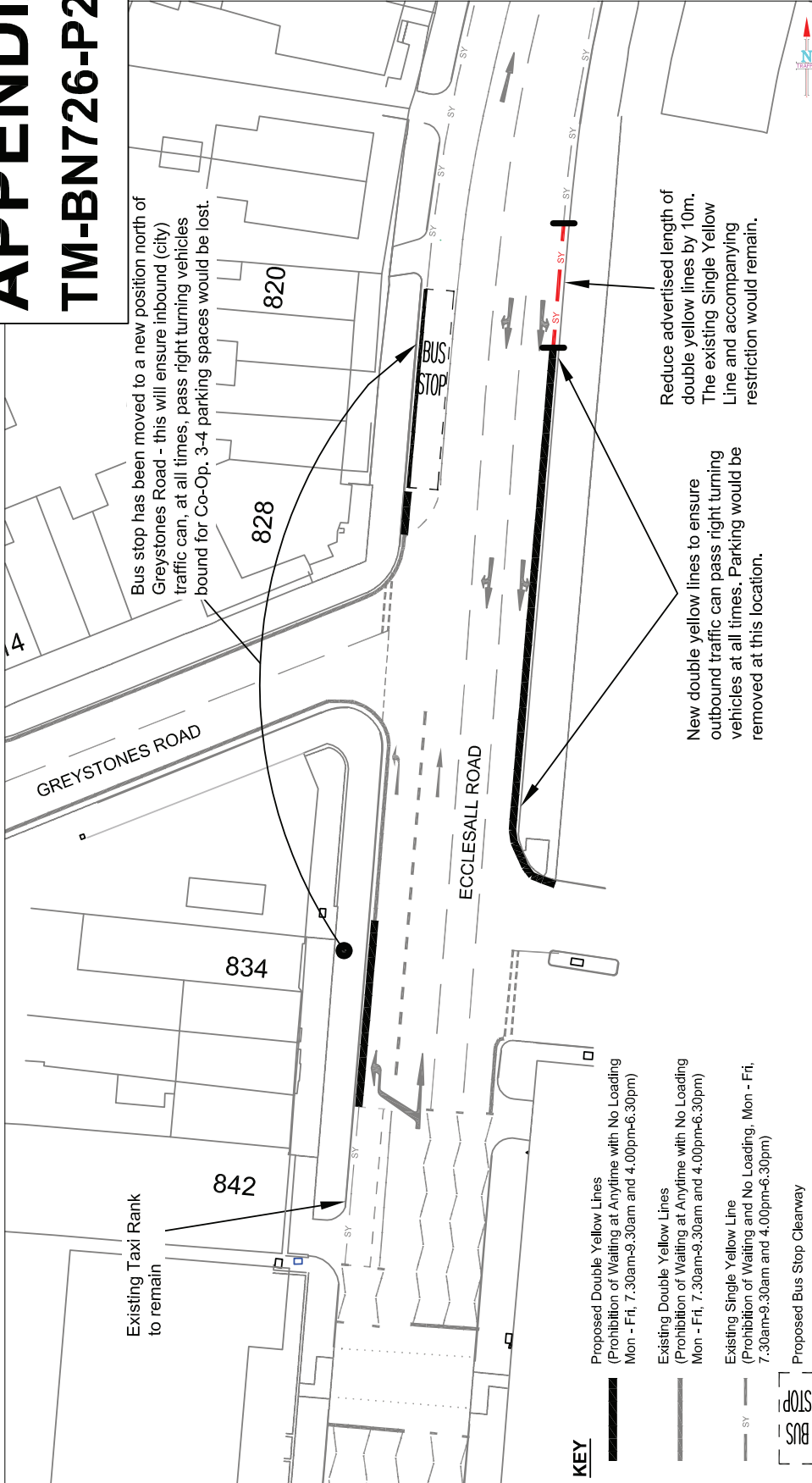
DRAWN: ML  
 CHECKED: Nov 11

Client: SHEFFIELD CITY COUNCIL  
 Scheme: Ecclesall Smart Route  
 Drawing Title: Rustlings Road to Greystones Road Traffic Regulation Order

Drawing No. TM-BN721-07-TRO  
 Scale: 1:500  
 Date: Nov 2011  
 SHEFFIELD City Council  
 TRAFFIC MANAGEMENT

# APPENDIX A

## TM-BN726-P2-TRO



<b>DEVELOPMENT SERVICES</b> <b>TRANSPORT &amp; HIGHWAYS DIVISION</b> <b>TRAFFIC SECTION</b> 2-10 CARBROOK HALL ROAD SHEFFIELD, S9 2DB Tel. 0114-273-6175. Fax. 0114-273-6182 E-mail Traffic.Management@Sheffield.gov.uk Development Services Director: L Sturch M.R., T.P.I.	Client <b>SHEFFIELD CITY COUNCIL</b>		Drawing No. <b>TM-BN726-P2-TRO</b>	Scale <b>1:500</b>	Date <b>NOV 2011</b>		TRAFFIC MANAGEMENT
	Scheme <b>Ecclesall Road Smart Route</b>						
Drawing Title <b>Ecclesall Road/Greystones Road Proposed Scheme</b>		Checked <b>JB</b> Nov 2011		Do not scale from this drawing Any errors/omissions to be reported immediately If in doubt, This drawing is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of Her Majesty's Stationery Office. © Crown Copyright. Unauthorised reproduction infringes Crown copyright and may lead to prosecution or civil proceedings. 1000148846-2010 DRAWN		SHEFFIELD CITY COUNCIL TRAFFIC MANAGEMENT	

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